

Division(s) affected: *Wheatley*

## **DELEGATED DECISIONS BY CABINET MEMBER FOR TRANSPORT MANAGEMENT**

**24 JUNE 2025**

### **WHEATLEY: CHURCH ROAD – PROPOSED ‘NO WAITING AT ANY TIME’ PARKING RESTRICTIONS**

**Report by Director of Environment and Highways**

#### **RECOMMENDATION**

**The Cabinet Member is RECOMMENDED to:**

- a) Approve the introduction of three additional sections of ‘No Waiting at Any Time’ on the north side of Church Road in Wheatley, as advertised.**

#### **Executive Summary**

1. This report presents responses received to a statutory consultation on proposals to introduce three additional sections of ‘No Waiting at Any Time’, joining up the existing parking prohibition (double yellow lines) at the following locations, which are illustrated in **Annex 1**:
  - a) adjacent to property no.37b - 12.5 metres in length,
  - b) adjacent to St Marys Church (opp. nos.70 & 72) - 10m
  - c) adjacent to St Marys Church (opp. the Merry Bells PH car park) - 12.5m.
2. In September 2024, as part of a wider review of on-street parking in Wheatley, new ‘No Waiting at Any Time’ (double yellow lines) were introduced on sections of the north side of Church Road (including outside St Marys Church) whilst retaining approximately six unrestricted parking spaces (locations as detailed in paragraph 1). These were provided to retain some parking to ease pressure upon the limited number of spaces, but also to act as a traffic calming feature to deter speeding. Double yellow lines between the unrestricted spaces provided ‘passing places’ to assist through traffic. The other restrictions introduced in Church Road, at the western end near the public car park and two bus stops, removed parking on both sides because severe congestion occurred regularly.
3. The parking team monitor all schemes after their implementation and displacement of parking is a regular occurrence, following new restrictions, although not always to locations that cause problems. In Church Road,

subsequent monitoring and feedback showed that parking had indeed displaced to the six parking spaces that were retained, contributing to access difficulties when there is oncoming traffic. The 'passing places' have proved to be inadequate when buses are forced to pull in to allow oncoming traffic through. Subsequently, requests have been received from the local County Councillor, Wheatley Parish Council, and Oxford Bus Company to address the delays caused to through traffic, especially buses.

4. Occasionally, funerals, weddings and other church activities take place that necessitate some parking taking place adjacent to the church. This is 'accepted' practice in that the church organisers contact the County Council's parking enforcement contractor, Trellint Ltd., through their control room, to give notice of the activity and enforcement officers take note accordingly. When attending a funeral, hearses (or other undertakers' vehicles) are exempt from yellow line restrictions. For other vehicles, accessing the church, parking is now more scarce and occasional parking on yellow lines contributes to obstructions to through traffic at these times.

## **Sustainability Implications**

5. The proposals were put forward to help improve the flow of traffic in the immediate vicinity, especially for larger vehicles including the regular scheduled bus services.

## **Financial Implications**

6. Funding for consultation on the proposals (and implementation if approved) has been provided by the CIL budget for parking schemes, received via South Oxfordshire District Council.

## **Legal Implications**

7. The consultation that has been undertaken complies with the consultation requirements for the various elements as required by law including under the Highways Act 1980, the Road Traffic Regulation Act 1984 and any other relevant legislation.
8. The scheme has been promoted by Oxfordshire County Council as the Highway Authority and Traffic Authority under the Highways Act 1980, and the Road Traffic Regulation Act 1984.

*Comments checked by:*

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## **Equalities and Inclusion Implications**

9. No negative implications in respect of equalities or inclusion have been identified in respect of these proposals.

## **Formal Consultation**

10. Formal consultation was carried out between 08 May and 30 May 2025. A notice was published in the Oxford Times newspapers, and an email was sent to statutory consultees & key-stakeholders, including Thames Valley Police, the Fire & Rescue Service, Ambulance service, Bus operators, countywide transport/access & disabled peoples user groups, South Oxfordshire District Council, Wheatley Parish Council, and the local County Councillor representing the Wheatley division.
11. Letters were sent directly to approximately 80 properties in the immediate vicinity, and public notices were also displayed on site adjacent to the proposals.
12. The parish council, and local Councillors (including County, District, Parish, Town) were also encouraged to use the consultation documents provided to publicise the proposals amongst local residents as necessary.
13. During the course of the formal consultation, 16 responses were received via the online survey, comprising of: three objections (19%), four partially supporting (25%), eight in support (50%), and one non-objection.
14. Additionally, a further three emails were received directly – with Thames Valley Police not objecting, Oxford Bus Company strongly supporting, and Wheatley Parish Council only partially supporting as they have concerns that there would be no parking available directly outside the Church for specific events, such as funerals and weddings.
15. The full responses are shown in **Annex 2**, and copies of the original responses are available for inspection by County Councillors. Any comments received that Officers identify as containing personal abuse and/or other personal information will be redacted as appropriate.

## **Officer Response to Objections/Concerns**

16. Many responses (including that from Wheatley Parish Council) are concerned about parking facilities for church activities. Whilst funeral vehicles and vehicles loading/unloading to the church are exempt from the yellow line restrictions, these activities are contributing to problems with access for through traffic. However, it is considered that the proposed removal of the approx. six spaces – meaning there will be no parking spaces marked on the north (Church) side of the road – will help on those occasions i.e. when church activities necessitate some parking on yellow lines by exempt vehicles.

17. The Parish Council has requested a space to be reserved for the church and commented that there is plenty of room for buses to get past. The difficulty with providing the space is that there is no such type of 'churchgoers only' parking restriction possible under national regulations, only by means of a time-limited bay or permit-holder bay. Unfortunately, that wouldn't offer any priority for churchgoers and in practice a space is unlikely to be kept clear.
18. It is therefore considered that retaining yellow lines (with the usual exemption for funeral vehicles and loading/unloading) is a more appropriate solution in this instance. Although there is just enough width for buses to pass a parked vehicle, it is the passage of through traffic that is compromised, and the manoeuvrability of buses is severely limited when both parking and oncoming traffic combine.
19. Some respondents have referred to parking problems further west along Church Street, between the recreation ground and the Sun Public House. New restrictions were introduced near the pub, at the junction with Templars Close, in September 2024, but there is still a 'pinch point' west of the recreation ground where parking reduces the road width making it too narrow for 2 vehicles to pass. This will continue to be monitored, and further restrictions can be considered if problems persist.
20. One respondent refers to a need to discourage through traffic. Whilst the parking review was not intended to affect through traffic, it is accepted that the proposed removal of the remaining spaces on the north side could negate any 'traffic calming' effect. In anticipation of this, a speed survey was undertaken in March 2025 and the average speeds recorded were 20.17mph eastbound and 21.99mph for westbound traffic, representing relatively good compliance with the 20mph speed limit. A further 'after' survey could be undertaken in due course.

**Paul Fermer**  
**Director of Environment and Highways**

Annexes	Annex 1: Consultation plan
	Annex 2: Consultation responses

Contact Officers: Mike Horton (Senior Officer – TRO & Schemes)  
James Whiting (Team Leader – TRO & Schemes)

June 2025

Drawing No.

TBC

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KEY

Existing No waiting at any time  
(double yellow lines)

Proposed No waiting at any time  
(double yellow lines)

Existing shared parking spaces  
Mon - Fri 9am - 4pm  
permit holders only or  
2 hours no return within 1 hour

Existing Limited Waiting  
Mon - Sat 8am - 6pm  
1 hour no return within 1 hour

CONSTRUCTION

CENTER 'NONE' IF APPLICABLE

MAINTENANCE/CLEANING

CENTER 'NONE' IF APPLICABLE

USE

CENTER 'NONE' IF APPLICABLE

DECOMMISSIONING/DEMOLITION

CENTER 'NONE' IF APPLICABLE

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Rev.	Date	Purpose of revision	Drawn	Checked	Approved

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Project title

Wheatley: Church Road  
Proposed Parking Restrictions

Drawing title

Changes to  
No waiting at any time  
(double yellow lines)

Drawing Status

Draft

Scale @ A4	Drawn by	Checked by	Approved by
Not to scale	MH	JW	JW

Date drawn	Date checked	Date approved
April 2025	April 2025	April 2025

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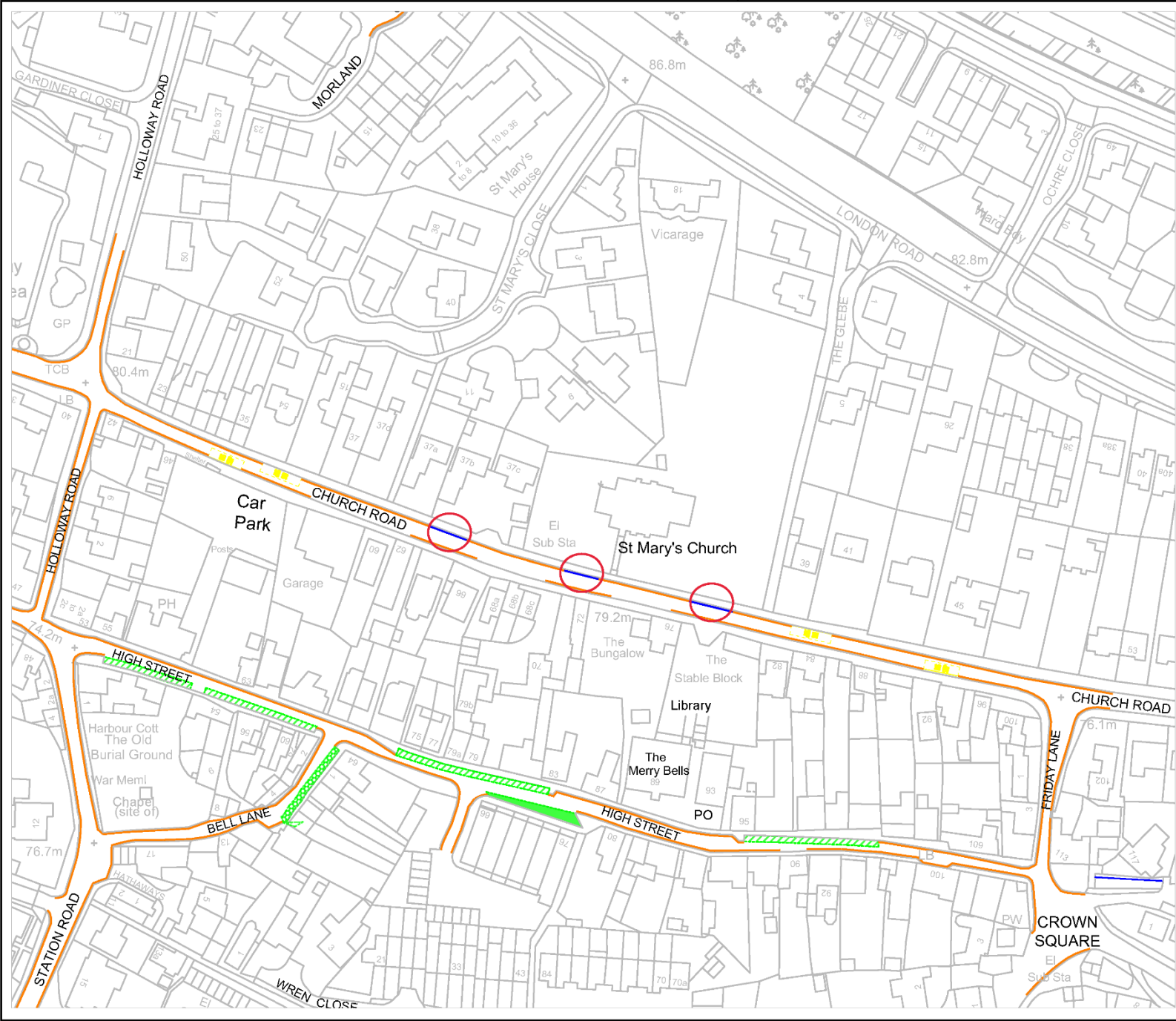
TBC

Drawing No.

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Revision

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## A. Email responses:

RESPONDENT	COMMENTS
(e1) Traffic Management Officer, (Thames Valley Police)	<b>No objection</b>
(e2) Managing Director, (Oxford Bus Company)	<p><b>Support</b> – As you would expect from our previous representations on this subject, Oxford Bus Company strongly supports these proposals.</p> <p>Church Road is an important corridor for buses, with 14 bus movements per hour between services 400 and 46 on Mondays to Fridays, and the current parking arrangements in Church Road are very difficult for buses to manoeuvre around, particularly when faced with oncoming traffic. This can cause unnecessary and, in many cases, excessive delays to bus services which impact upon the journeys of a large number of local residents.</p>
(e3) Wheatley Parish Council	<p><b>Partially support</b> – Wheatley Parish Council supports the amendments to the waiting restrictions but has one small area of concern. There will be no parking space outside the church. We would like to see a space reserved for church business including funerals, weddings etc and when vehicles need to unload while on business in the church. I note your point that any parking on that side of the road compromises accesses for buses, but we have looked at this in the past and there is plenty of room for a reserved space and for buses to get past.</p>

*B. Online responses:*

RESPONDENT	COMMENTS
(o1) As a business, (Wheatley, Church Road)	<b>Object</b> – As a business you have already sneaked lines in front premises and I don't want anymore
(o2) Local resident, (Wheatley, Holloway)	<p><b>Object</b> – If the removal of parking permission goes ahead people will be able to drive through church road without any restrictions.</p> <p>We need to discourage people from driving through the village and not encourage them by making it easier. The council need to decide whether you want less traffic or more traffic.....</p>
(o3) Local resident, (Wheatley)	<b>Object</b> – Get up of so many parking restrictions, it prevents people popping into shops
(o4) Local resident, (Wheatley, Church Road)	<p><b>Partially support</b> – I support the need to remove the "unrestricted" parking provision on Church Road with the provision that there is some form of exemption for.....</p> <p>(a) hearses to park in front of St Mary's when there is a funeral service (b) vans etc to park in front of St Mary's to unload materials needed for events held in the church.</p>
(o5) Local resident, (Wheatley, Church Road)	<b>Partially support</b> – The proposal for the places on one side church road seems sensible. My concern the council is chipping away at the parking for my visitors. I live on own, now with road restrictions and car park has more restrictions including Sunday. It stops friends visiting me.
(o6) Local resident, (Wheatley, Church Road)	<b>Partially support</b> – We fully support the removal of the parking adjacent to 37b and the one at the eastern end of the churchyard. We do understand the impossible situation for buses when these spaces are used for parking. We have two problems:

	<p>1. The church needs somewhere for hearses and for bridal cars and other downloads (e.g florists) to stop legally, even for a few minutes, even if always attended, so would ask to keep the parking zone opposite 70/72, specified for purposes related to the church (I am not a member of the congregation)</p> <p>2. The local area plan aims to increase parking around the village and this proposal inevitably reduces it. Therefore, to address the issues of keeping the space opposite 70/72</p> <p>a) the parking space itself could be a little shorter but more importantly</p> <p>b) the parking outside 76 could be moved east (rather than reduced), to bring it as close to the Merry Bells/library car park as is legal</p> <p>This will still reduce the parking available, but would show some awareness of local issues.</p>
(o7) Local resident, (Wheatley, Church Road)	<b>Partially support</b> – I agree with the proposed changes, Double yellow lines also need introducing at the Sun pub end of church road. Lots of cars have been damaged here and it is a significant pinch point for dustcarts and bigger vehicles
(o8) Local resident, (Wheatley, Church Road)	<b>Support</b> – Heavy traffic on road and a busy bus route. The parking in those areas proves very difficult for large vehicles to pass and can cause a standstill of traffic.
(o9) Local resident, (Wheatley, Church Road)	<b>Support</b> – Allowing parking on the Church side causes absolute traffic chaos given how narrow the 'chicanes' have been made. And cars parked on the opposite side to the Merry Bells car park entrance also partially block cars entering and leaving. Only a matter of time before a serious crash happens along this stretch.
(o10) Local resident, (Wheatley, Templars Close)	<b>Support</b> – Buses are frequently delayed by cars parked on these sections of Church Road. Unless buses can be re-routed along London Road then these additional parking restrictions are necessary.
(o11) Local resident, (Wheatley, Church Road)	<b>Support</b> – Drivers parking on the north side of Church Road restrict the possibility of the buses getting through. Parking spaces on the south side are often taken up with cars parking all day and overnight . This means that



	people attending the Church have limited parking. Particularly disabled people. I suggest that there should be a time limit on parking .
(o12) Local resident, (Wheatley, Church Road)	<b>Support</b> – When cars are parked on the north side of the road opposite tripp and 64 the buses can't get thought without driving on the pavements. This has already damaged them,
(o13) Local resident, (Wheatley, Church Road)	<b>Support</b> – It is a low cost / high impact measure on traffic alleviation. 1 or 2 parked cars in those areas during peak times is enough to cause large traffic jams on Church road that spill over the surrounding streets.
(o14) Local resident, (Wheatley, Church Road)	<b>Support</b> – Traffic along church road is terrible and made much worse when cars are parked in the free spaces! No room for a fire engine or ambulance then
(o15) Local resident, (Wheatley, Old Road)	<b>Support</b> – I cycle this route regularly and cars maneuvering around the parked cars is often problematic, especially if there's a bus around. I'd also like to see more double yellow lines along between the play area and the pub, as cars often mount the pavement to pass each other here and it's a busy pavement right by a play area, especially at school times.
(o16) Local resident, (Wheatley, Barlow Close)	<b>No objection</b> – There is an equally important parking problem between the Rec and the Sun which urgently needs addressing